

**BEFORE THE CALIFORNIA PUBLIC UTILITIES COMMISSION OF THE
STATE OF CALIFORNIA**

Application of the County of Santa Clara for an order)
authorizing modification of a grade separated crossing of)
Lawrence Expressway by the Peninsula Corridor Joint) APPLICATION NO
Powers Board main commuter rail line mile post 40.78 and)
Southern Pacific spur rail line in the city of Sunnyvale, California.)

APPLICATION

The petition of the COUNTY OF SANTA CLARA respectfully shows that:

1. The exact legal name of the Applicant is the COUNTY OF SANTA CLARA, TRANSPORTATION AGENCY.
2. Correspondence in regard to this application should be addressed to:

Steven M. Woodside, County Counsel
Kevin D. Allmand, Deputy County Counsel
County Government Center, East Wing
West Hedding Street, San Jose, CA 95110
TEL. (408) 299-2111
3. This application and the relief sought herein are Section 1201 through 1205 inclusive of the Public Utilities Code of the State of California.
4. The California Public Utilities Commission (CPUC) has been furnished a complete copy of the Initial Study of Environmental Assessment (IS/EA) for the Lawrence Commuter Lane Project. This IS/EA describes the preferred alternatives and supports the proposed crossing modification. The public need to be served by the proposed project is described in the IS/EA and summary, and the preferred alternative of the IS/EA.
5. The Peninsula Corridor Joint Powers Board, herein sometimes referred to as PC-JPB is a Board created as a common carrier operating a standard gauge railroad system in the State of California and said board owns and operates the main commuter rail line at the location of the project herein proposed to be constructed.
6. The Southern Pacific Transportation Company (SPTC) is a corporation doing business as a common carrier operating a standard gauge railroad system in the State of California and said corporation owns and operates railroad spur track at the location of the project herein proposed to be constructed.

Approved Accepted Adopted Denied Presented

BY THE BOARD OF SUPERVISORS
OF THE COUNTY OF SANTA CLARA
Phyllis A. Perez, Clerk of the Board

By: Erlene Jones
Deputy Clerk

Date: AUG 10 1993

7. The purpose of the project is to: Widen the existing crossing at separated grades to be known as the "SUNNYVALE (SPRR) OVERHEAD" above the PC-JPB main line and SPRR spur line in the city of Sunnyvale. This is part of a construction project to accomodate High Occupancy Vehicles (HOV) on Lawrence Expressway, between State Route 237 and State Route 280.
8. The project is scheduled for construction early in 1995. It is expected that the construction will be completed by late summer 1996.
9. The following exhibits are transmitted:
 - a. One copy of Exhibit "A"; three (3) typed pages, giving general information and specific data.
 - b. One print of Exhibit "B"; three (3) sheets, showing Location map, Site plan, and General plan with structure plan, elevation and views of the proposed overhead construction.
 - c. One copy of Exhibit "C"; right of way plans and legal description for PC-JPB & SPRR for the project site.
 - d. One copy of Exhibit "D"; the cover sheet and of the signature page for the Final Environmental Impact Statement.

The complete IS/EA is available for review at the Transportation Agency, 3331 North First Street, Building "B", 2nd.fl., San Jose, CA 95134. If so desired, please call Project Engineer, Gamini Rajapakse, at (408) 321-7144.

10. Copies of the enclosed exhibits have been sent to the PC-JPB & SPTC. It is anticipated that the project will be financed with State, Federal and Local funds.
11. The Transportation Agency, is the lead agency for this project. The project has been developed so as to give detailed consideration to the potential impact upon the quality on the environment. On August 18, 1986, in compliance with the Public Resources Code, the Federal Highway Administration approved the Final Environmental Assesment and issued a Finding of no Significant Impact for the project as shown by Exhibit "D" and made a part hereof.

WHEREFORE, applicant requests that:

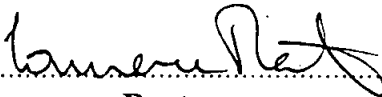
a. The Public Utilities Commission of the State of California issue its authorization for said construction permit pursuant to provisions under section 1201-1205, inclusive, of the Public Utilities Code.

b. The Commission find that the project will not have a significant effect on the environment, the mitigation measures adopted by the Transportation Agency, as set forth in Exhibit "D" are satisfactory.

c. The order provides four (4) years from the date of such order within which to complete the proposed project.

Dated at San Jose, California, this10..... day of August, 1993

Respectively submitted,
COUNTY OF SANTA CLARA

By 

Lawrence Reuter
Director
County of Santa Clara
Transportation Agency
3331 North First Street,
San Jose, CA 95134

By 

Kevin D. Allmand
Deputy County Counsel
County of Santa Clara
County Government Center, East Wing
West Hedding Street,
San Jose, CA 95110-1770
(408) 299-2111

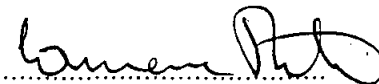
VERIFICATION

LAWRENCE REUTER under penalty of perjury, declares:

That he is the Director of the Transportation Agency of the County of Santa Clara and is authorized to make this verification on behalf of the Santa Clara County Transportation Agency; that he has read the foregoing application and knows the contents thereof; that he is informed and believes the matters herein, stated to be true, and on that ground alleges that the matters stated herein are true.

I declare under penalty of perjury that the foregoing is true and correct.

Executed at San Jose, California on August 10, 1993.



LAWRENCE REUTER
Director

PROOF OF SERVICE BY MAIL

**State of California
County of Santa Clara**

I am now, and at all times herein mentioned have been, a citizen of the United States, over the age of eighteen (18) years, a resident of Santa Clara County, California, and not a party to the within action or cause; that my business address is 3331 North First Street, Building 'B', 2nd. fl., San Jose, CA 95134; that I served a copy of the application to the California Public Utilities Commission for the modification of the existing grade seperated crossing of Lawrence Expressway and the Peninsula Corridor Joint Powers Board main line and Southern Pacific spur rail line in the city of Sunnyvale, by placing a copy in an envelope addressed to each of the following:

Mr. Tomas Lewcock
City Manager
City of Sunnyvale
456 West Olive Avenue
Sunnyvale
CA 94088-3707

Mr. Preston W.Kelly
District IV, Director
State of California
Department of Transportation
Box 23660
Oakland, CA 94623-0660

Ms. Sally Reed
County Executive
County of Santa Clara
70 West Hedding Street
San Jose, CA 95110

C.J.Burroughs
Chief Engineer
Southern Pacific Transportation Company
One Market Plaza, Room 1001
San Francisco, CA 94105
ATTN: Dick Dahllof

Tom Davids
Property Manager
Peninsula Corridor Joint Powers Board
1250 San Carlos Avenue
San Carlos, CA 94070-1306

Which envelopes were then sealed and, with postage fully prepaid thereon, was on ~~August 13, 1993~~ August 13, 1993 deposited in the United States mail at the place so addressed, or that there is regular communication by mail between the place of mailing and the place so addressed.

I declare under penalty of perjury that the forgoing is true and correct.

Executed this13..... day of August...1993, at San Jose, California.


GAMINI RAJAPAKSE

EXHIBIT LISTING

Modification of the existing grade separated crossing of Lawrence Expressway and the PC-JPB main commuter rail line and SPRR spur rail line.

EXHIBIT

DESCRIPTION

- | | |
|---|--|
| A | Three (3) typed pages, giving general information and specific data. |
| B | Location map, Site plan & General Plan |
| C | R/W map & Legal description for PC-JPB & SPRR parcels. |
| D | Final Environmental Impact Statement, cover page and the signature page. |

EXHIBIT "A"

I. GENERAL INFORMATION

The County of Santa Clara, Transportation Agency proposes to construct two commuter (High Occupancy Vehicle or HOV) lanes on Lawrence Expressway between State Route 237 in the north and I-280 in the south. In order to accommodate the HOV lanes on the expressway, the project will include the widening of four (4) bridges. Bridges at Stevens Creek Blvd. and El Camino Real, will be widened in the median. The Sunnyvale (SPRR) Overhead will be widened on the east side. Central Expwy./ Lawrence will be widened on both sides by adding retaining walls.

Proposed crossing modification in the City of Sunnyvale, County of Santa Clara, Sunnyvale (SPRR) Overhead at Lawrence Expressway will be widened by 18'-2" on the east side of the bridge. This structure crosses at separated grades over the west bound & east bound main tracks of Peninsula Corridor Joint Powers Board (PC-JPB) and Southern Pacific Transportation Company (SPTC) spur track.

II. SPECIFIC DATA

A. MODIFICATION OF AN EXISTING GRADE SEPARATION

1. The existing structure known as "SUNNYVALE (SPRR) OVERHEAD" at Lawrence Expressway, Br. No. 37C-198, cross over the PC-JPB'S & SPTC'S tracks at approximate railroad mile post 40.78 of the main line as shown on Exhibit "B".

(a). The existing overhead structure consist of three (3) spans, 112, 132 & 112 foot long. It is a built up steel girder bridge with concrete deck supported by columns on a common pile cap foundation. The deck accommodates three (3) eleven (11) foot lanes in each direction with pedestrian access on the east side of the bridge.

(b). The proposed widening will be done by adding two (2) built up steel girders and concrete deck. Girders will be supported by extension of the existing bent on pile foundation. The new deck will accommodate three (3) eleven (11) foot lanes and one (1) fifteen (15) foot lane and a five (5) foot wide sidewalk in each direction.

(c). In addition to bridge widening, there will be seismic retrofit modification to the existing bent and if required to the foundation. We are still in early seismic analysis stage and have not identified all the necessary seismic retrofit work.

EXHIBIT "A"

- (d). Bent # 2 foundation excavation will be outside the required 10 ft. clearance from center line of the east bound track. Piles will be driven to support the bent foundation. New bent will be tied into the existing bent by dowels. As part of retrofit work existing bent openings will be filled in.
- (e). Bent # 3 foundation excavation will be in the existing station parking area. Piles will be driven to support the bent foundation. New bent will be tied in to the existing bent by dowels. As part of retrofit work existing bent openings will be filled in.
- (f). The bridge (portions) removal will be done in two stages. East side portion will be done first, and west side will be last to permit the continuing traffic on the structure during construction.
- (g). The current Average Daily Traffic (ADT) on the structure is about 71,000 as measured in May 1992. On the proposed widened bridge structure, the 2010 projected ADT is estimated to be over 100,000.
- (h). Current daily rail traffic consist of approximately 60 passenger trains and freight trains at a maximum speed of 50 miles per hour. Railway traffic will continue to use the existing tracks during construction.
- (i). Existing vertical clearance of twenty three (23) feet will be maintained in the PC-JPB main line and twenty two (22) foot nine (9) inches will be maintained in the spur line.

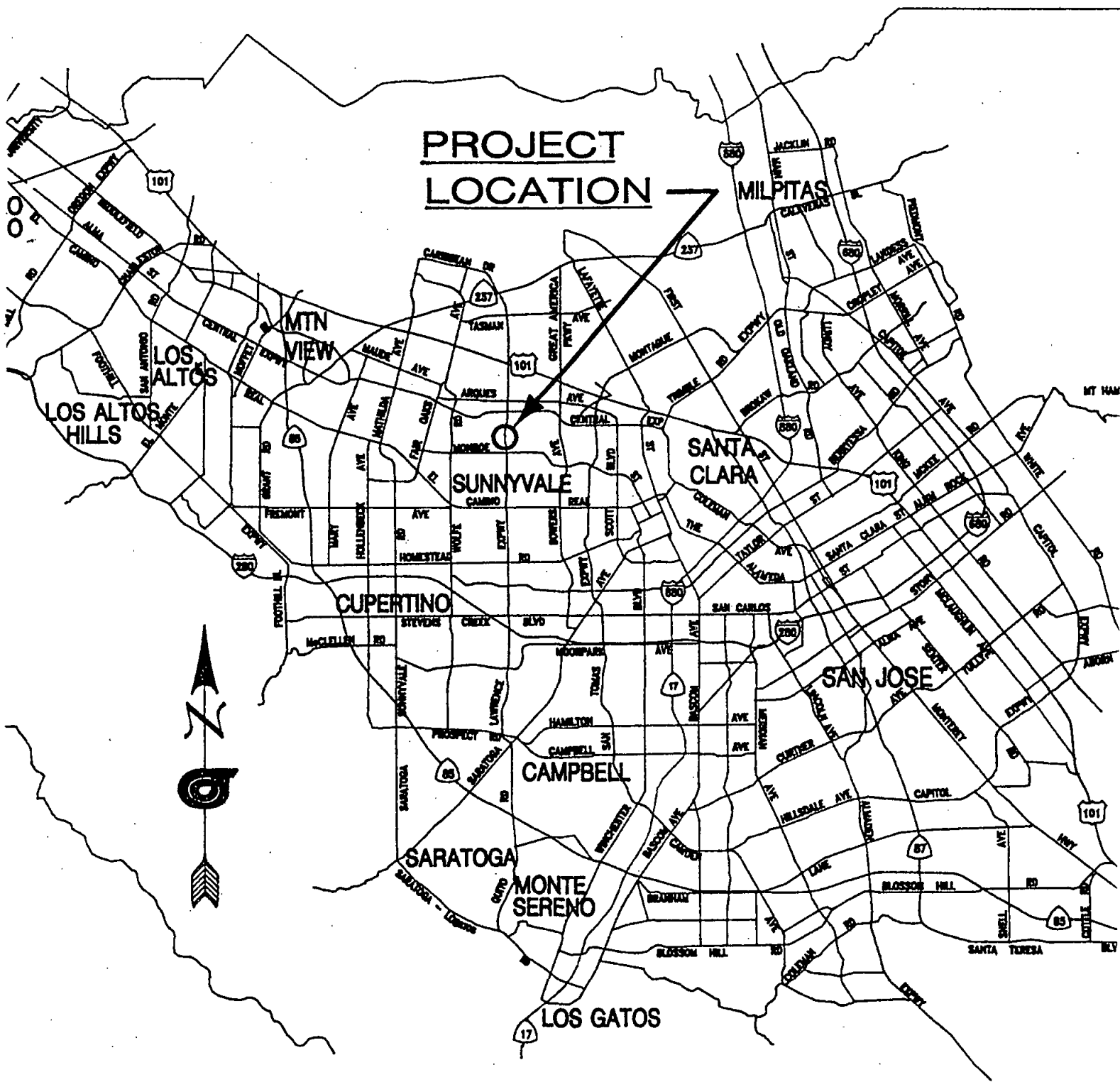
Flagging will be required during the following operations during construction:

Excavation for Bent # 2 & pile driving	2 wk.
Demolition (bridge portion) - east side	2 wks
Demolition (bridge portion) - west side	1 wk.
Girder erection over the tracks in spans	1wks.
Falsework for concrete deck	2 wks.
During concrete pouring	1 wk.
Removal of falsework	1 wk.
Total (estimate)	10 wks.

EXHIBIT "A"

Estimated schedule is as follows:

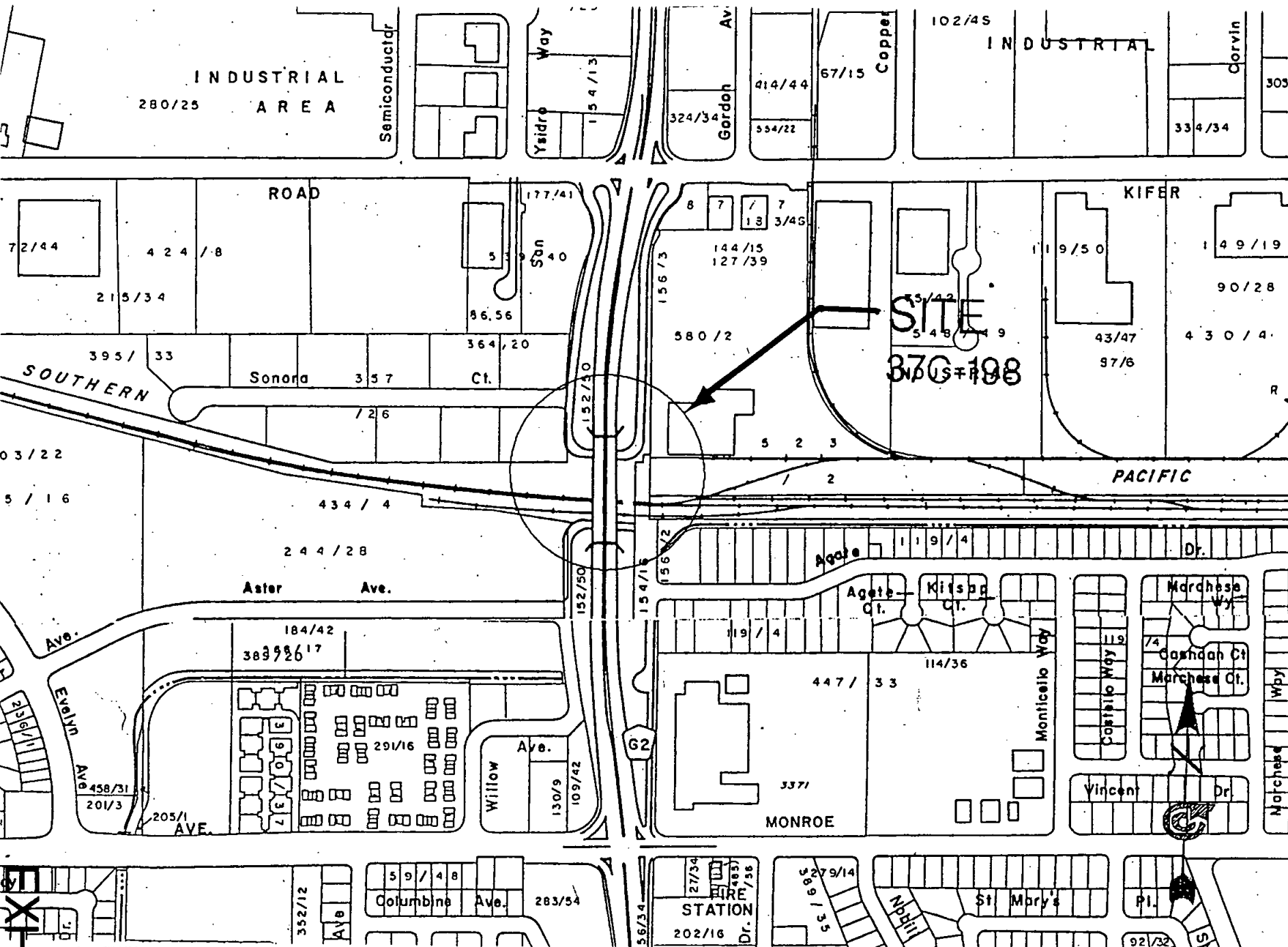
Preliminary - General plan	3/93
Retrofit strategy w/Caltrans	10/93
100% PS&E	2/94
Construction begins	3/96



**PROJECT
LOCATION**

LOCATION MAP

1" = 2.50 MILES



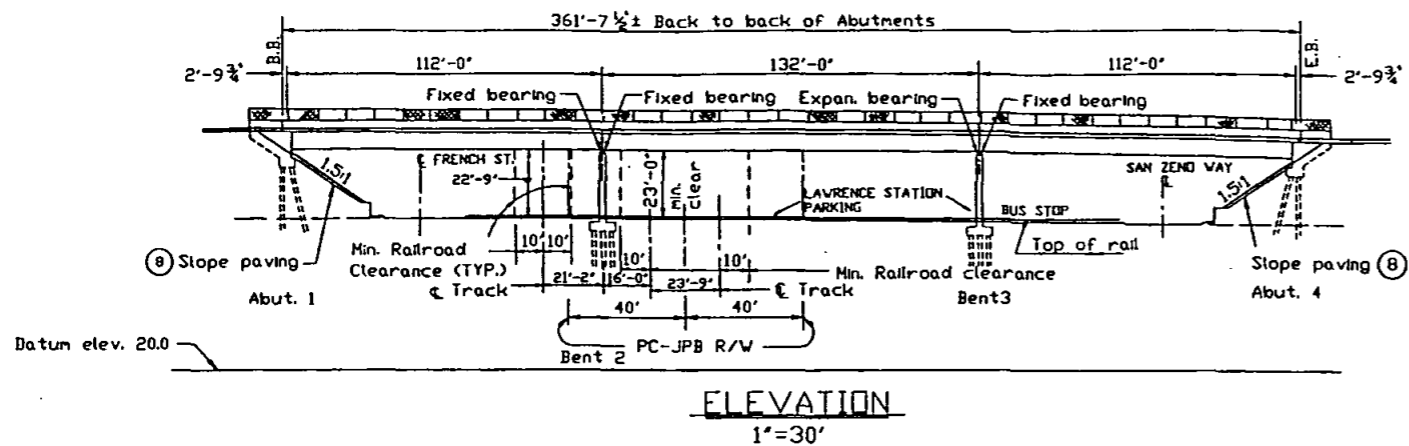
SITE PLAN

SCALE: 1" = 500'

EXHIBIT B

B-2

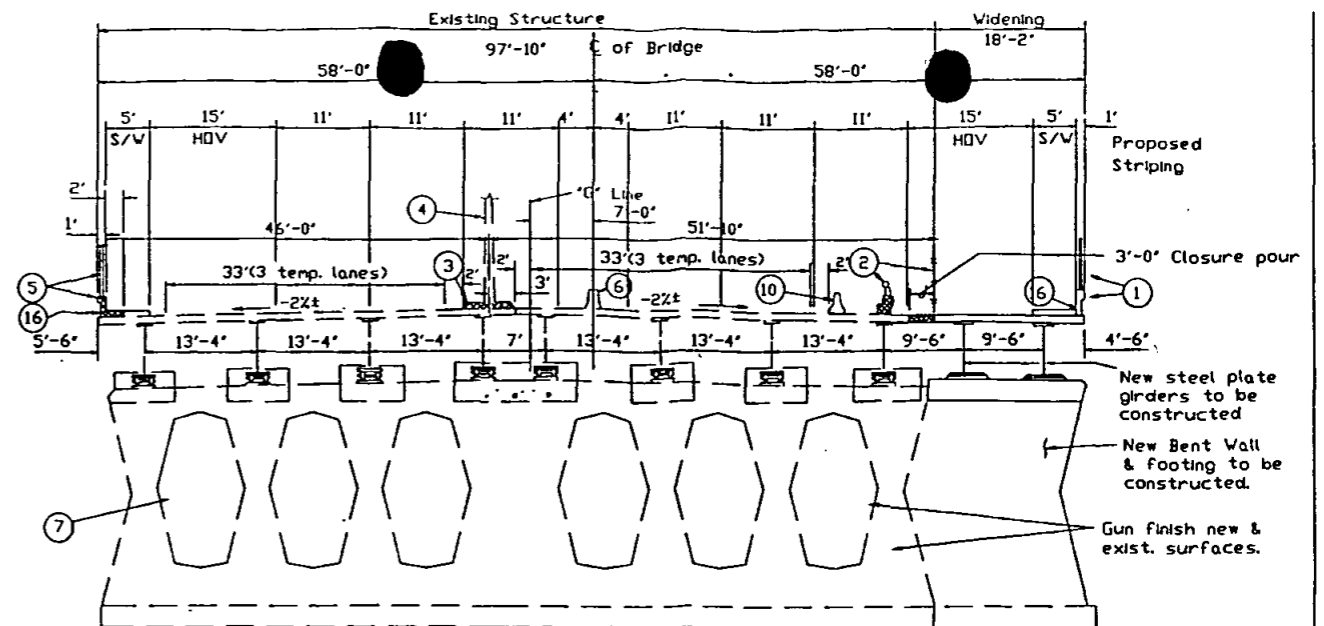
Conform to existing deck profile
PROFILE



Concrete Pile Class 70		
Location	Specified Tip Elev.	Probable Tip Elev.
Abut 1	+7.0	+2.0
Bent 2	0.0	-5.0
Bent 3	-3.0	-8.0
Abut 4	0.0	-5.0

All piles shall be driven in pre-drilled holes to elevation 30.0.

Some very high driving resistance may be expected above specified tip elevations.



Indicates portion of bridge to be removed.

STANDARD PLAN

(STANDARD PLANS LISTED ARE THE CALIFORNIA DEPARTMENT OF TRANSPORTATION STANDARD PLANS DATED JULY, 1992 WHICH APPLY TO THIS CONSTRUCTION).

AG2-C	EXCAVATION AND BACKFILL-BRIDGE
A75-A	CONCRETE BARRIER TYPE 50
A77C-2	METAL BEAM GUARD RAILING
A79-C	GUARD RAIL CONNECTIONS TO BRIDGE RAILS, RETAINING WALLS AND ABUTMENTS
B0-1	BRIDGE DETAILS
B0-3	BRIDGE DETAILS
B0-5	BRIDGE DETAILS
B0-13	BRIDGE DETAILS
B2-5	PILE DETAILS-CLASS 45 OR CLASS 70
B6-21	JOINT SEALS
B11-30	TEMPORARY RAILING (TYPE K)
B11-S2	CHAIN LINK RAILING TYPE 7
B11-S3	CONCRETE BARRIER TYPE 26

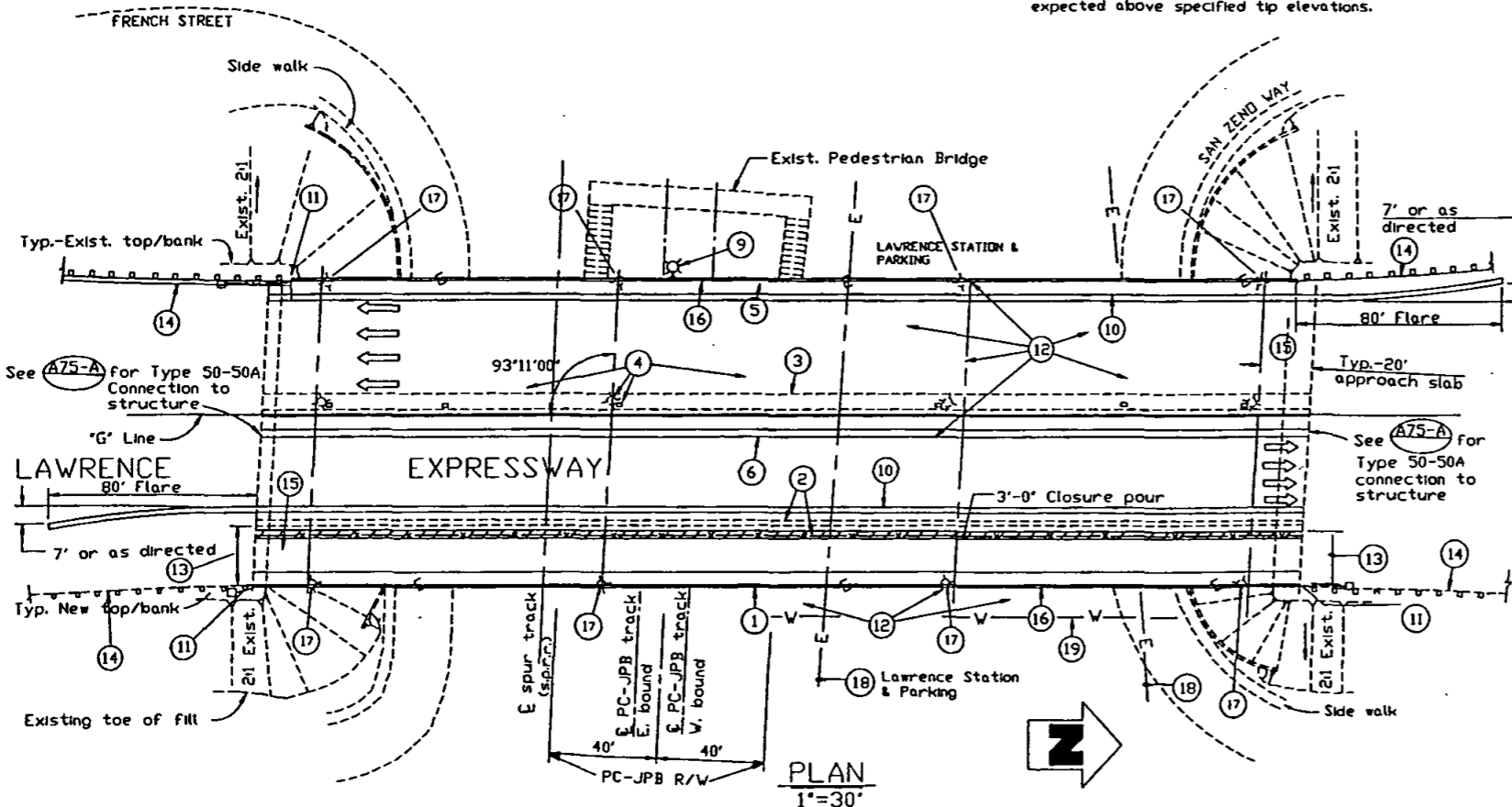
APPROXIMATE QUANTITIES

Mobilization	LUMP SUM
CONSTRUCTION AREA SIGNS	LUMP SUM
TEMPORARY RAILING (TYPE K)	940 LF
TRAFFIC CONTROL SYSTEM	LUMP SUM
PAINT TEMPORARY TRAFFIC STRIPES	1200 LF
SALVAGING MATERIALS FROM EXISTING FACILITIES	LUMP SUM
STREET REMOVALS AND REPLACEMENTS	LUMP SUM
BRIDGE REMOVAL (PORTION)	LUMP SUM
REMOVE CONCRETE	LUMP SUM
CLEAR AND GRUB	LUMP SUM
FURNISH PILING (CLASS 70)	1960 LF
DRIVE PILE (CLASS 70)	32 EA
DRILL AND BOND BOWELS	3,500 LF
JOINT SEAL (TYPE AL)	800 LF
REFINISH BRIDGE DECK	1,440 LF
CLEANING AND PAINTING STRUCTURAL STEEL	LUMP SUM
ASPHALT CONCRETE (BRIDGE)	330 SY
MODIFY EXISTING ELECTROLIER	LUMP SUM

FINAL PAY QUANTITIES

STRUCTURE EXCAVATION	220 CY
STRUCTURE BACKFILL	120 CY
STRUCTURAL CONCRETE (APPROACH SLAB)	35 CY
STRUCTURE CONCRETE BRIDGE FOOTING	52 CY
STRUCTURE CONCRETE BRIDGE	560 CY
BAR REINFORCING STEEL	150,000 LBS
STEEL STRUCTURES	280,000 LBS
ERECT STRUCTURAL STEEL	280,000 LBS
MISCELLANEOUS METAL (EXPANSION) JOINT ARMOR)	80 LF
MISCELLANEOUS METAL (RESTRAINER)	970 LBS
METAL BEAM GUARD RAILING (WOOD POST)	250 LF
CONCRETE BARRIER (TYPE 25)	784 LF
CONCRETE BARRIER (TYPE 50A)	400 LF
SLOPE PAVING (CONCRETE)	140 CY
CORE AND PRESSURE GROUT	LS70 LF

STATE BRIDGE NO: 37C-19B



NOTES:

- 1 CONSTRUCT CONCRETE BARRIER TYPE 26 (B11-S4) AND 6' CHAIN LINK RAILING TYPE 7 (B11-S2)
- 2 REMOVE EXISTING CONCRETE BARRIER AND FENCE.
- 3 REMOVE EXISTING MEDIAN CURB & REFINISH DECK.
- 4 REMOVE EXISTING ELECTROLIERS AND PULL BOXES.
- 5 REMOVE AND SALVAGE EXISTING METAL RAIL. CONSTRUCT TYPE 26 BARRIER AND CONSTRUCT CHAIN LINK RAILING TYPE 7 (B11-S4)
- 6 CONSTRUCT CONCRETE BARRIER TYPE 50A (A75-A)
- 7 RETROFIT EXISTING COLUMN BY CONSTRUCTING REINFORCED CONCRETE INFILLS IN ALL PIERS.
- 8 CONSTRUCT SLOPE PAVING.
- 9 EXISTING ELECTROLIER TO REMAIN AND WIRING TO REMAIN.
- 10 INSTALL TEMPORARY TYPE "K" RAILING (940± LF) (A79-C)
- 11 MBGR CONNECTION TO BRIDGE BARRIER.
- 12 PROVIDE OPEN JOINTS IN BARRIERS WHEREVER JOINTS IN DECK SLAB EXIST. JOINT WIDTHS TO MATCH DECK JOINT WIDTHS.
- 13 CONSTRUCT NEW 20' LONG APPROACH SLAB.
- 14 METAL BEAM GUARD RAILING (WOOD POST). SEE ROADWAY PLAN
- 15 PAINT 'SUNNYVALE OVERHEAD' BRIDGE NO. 37C-19B (A77C-2)
- 16 INSTALL NO.9 & NO.5(T) PULL BOXES & 2" CONDUIT, TWO EACH ON EACH SIDE. (ES-7D) (ES-7C) (ES-8)
- 17 INSTALL ELECTROLIERS & PULL BOXES
- 18 EXISTING UNDER BRIDGE ELECTRICAL LINE
- 19 EXISTING 10" WATER LINE

STANDARD DR PLAN SHEET NO. (Symbol)

DETAIL NO. (Symbol)

30% DESIGN PLAN

G.R. JAN 1993 C3475

R.K. JAN 1993

ACAD FILE: LVRGP.DWG

SUNNYVALE (SPRR) O.H. (WIDEN)/LAWRENCE EXPWY.

GENERAL PLAN

3 16

EXHIBIT B

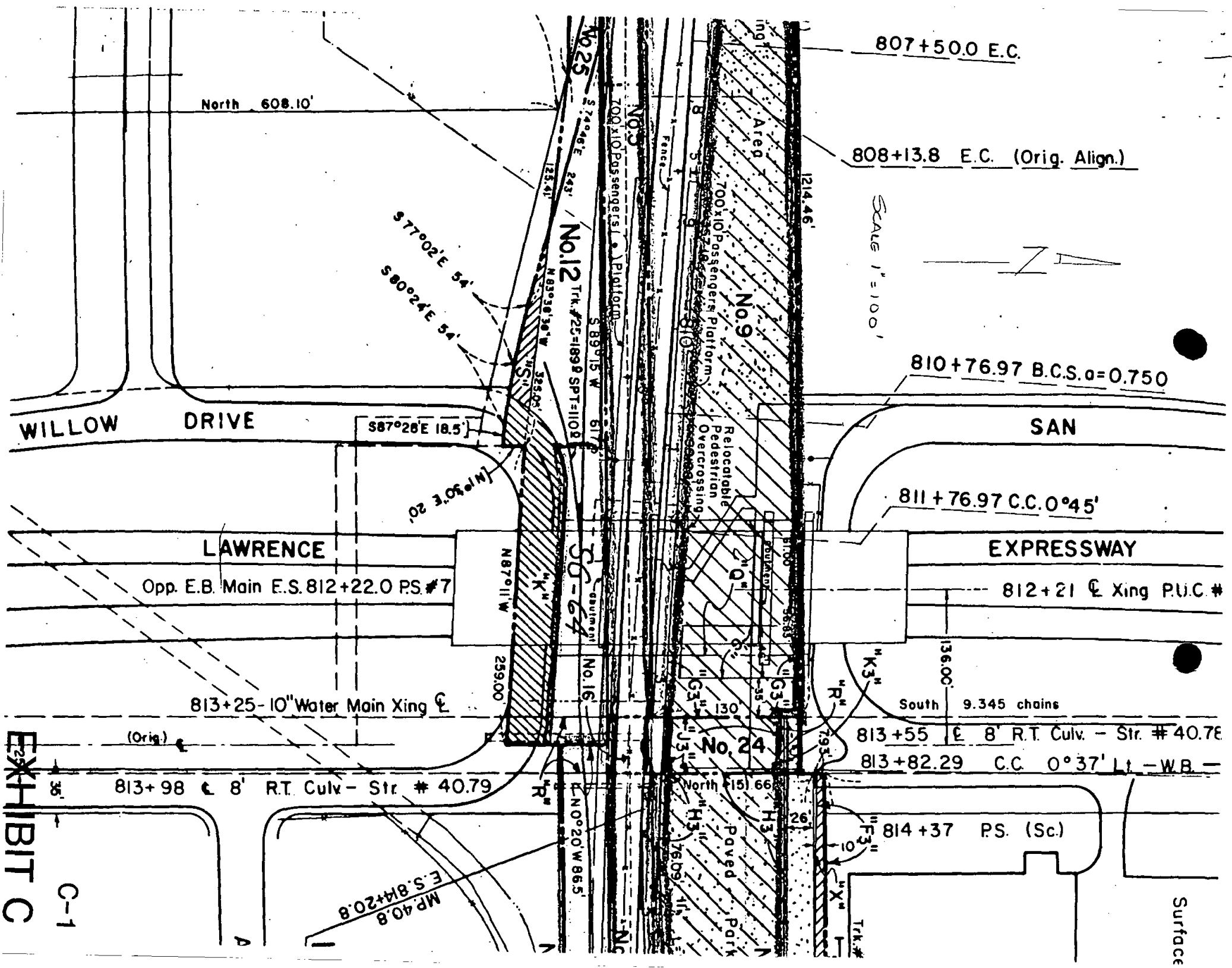


EXHIBIT C

C-1

Surface

Order No. 990006/SC-60 2ND AMENDED

THE LAND REFERRED TO HEREIN IS SITUATED IN THE CITY OF SUNNYVALE, COUNTY OF SANTA CLARA, STATE OF CALIFORNIA AND IS DESCRIBED AS FOLLOWS:

A portion of the land described in the "second" described parcel of land, as contained in the Orders of Condemnation had in the District Court and for the County of Santa Clara, State of California entitled, "the San Francisco and San Jose Railroad Company, a corporation, Plaintiff vs. Harry Love, et al, Defendants". The Orders of Condemnation were issued July 26, 1864 and September 16, 1864 and recorded September 16, 1864 in Book B of Misc., at Page 219, Santa Clara County Records, described as follows:

A strip of land of a uniform width of 40 feet, lying equally 20 on each side of the following described line:

BEGINNING at the intersection of the central line as described in said Orders of Condemnation, with the northwesterly line of the lands of the Enright Tract, as confirmed to James Enright by the United States of America in the patent recorded April 17, 1866 in Book A of Patents, Page 79; thence westerly along said central line, 5324 feet to the easterly line of Rancho Pastoria de Las Borregas, as confirmed to Mariano Castro by the United States of America in a patent recorded March 28, 1866 in Book C of Patents, Page 228, Santa Clara County Records.

EXCEPTING THEREFROM that portion lying within the lands conveyed to J. J. & W. Company, Inc., by deed recorded January 24, 1979 in Book E 246 of Official Records, at Page 145, described as follows:

BEGINNING at the most westerly corner of land described in deed dated December 3, 1901, from A. W. Mott, et ux., to Southern Pacific Railroad Company, recorded December 6, 1901, in Book 247 of Deeds, at Page 234, Santa Clara County Records; said corner being at an angle point in the northerly line of Parcel "C", as said parcel is shown upon that certain Record of Survey Map filed for record on November 6, 1968 in Book 244 of Maps, at Page 28, Santa Clara County Records; thence South 67° 33' 00" East, along the southwesterly line of land described in said deed, being also along said northerly line of Parcel "C", 276.00 feet; thence South 86° 28' 45" East continuing along said northerly line of Parcel "C", 262.90 feet; thence North 3° 31' 15" East 45.88 feet; thence North 81° 43' 00" West 525.789 feet to the point of beginning.

Order No. 990006/SC-61 1ST AMENDED

THE LAND REFERRED TO HEREIN IS SITUATED IN THE CITY OF SUNNYVALE, COUNTY OF SANTA CLARA, STATE OF CALIFORNIA AND IS DESCRIBED AS FOLLOWS:

All that certain lot, piece or parcel of land situate, lying and being at Lawrence Station, County of Santa Clara, State of California, and bounded and particularly described as follows:

COMMENCING at a point in the westerly line of the Lawrence Station Road, where said line intersects the northern boundary of the right of way of the Southern Pacific Railroad Company, opposite engineer's station 813 + 60 or thereabouts; thence north along said westerly line 130 feet; thence west 1225 feet, more or less to a point 50 feet distant at right angles from the northern boundary of said Southern Pacific Railroad right of way; thence northwesterly parallel to and 50 feet distant from said northern boundary of right of way 1500 feet, more or less to the western boundary of Lot 2 of Map made by Pettrich P. Parker, surveyors in March, 1891 and recorded in Book "E" of Maps, at Page 91, Santa Clara County Records; thence South $10^{\circ} 30'$ West, 50 feet to a point in the northern line of right of way of the Southern Pacific Railroad Company, opposite and 20 feet distant from center line of said Southern Pacific Railroad's track at engineer's station 786 + 25 or thereabouts; thence easterly along said northern line of right of way 2740 feet, more or less to the point of beginning.

Being the same lands as conveyed by Mrs. Sarah E. Houston to Southern Pacific Railroad Company, a corporation, by deed recorded March 1, 1898 in Book 203 of Deeds, Page 442, Santa Clara County Records.

EXCEPTING THEREFROM that portion lying within the parcel of land conveyed to the State of California, by deed recorded July 26, 1984 in Book I 737 of Official Records, at Page 250, Santa Clara County Records, described as follows:

BEGINNING at the point of intersection of the northerly line of land described in deed dated February 10, 1898 from Mrs. Sarah E. Houston to Southern Pacific Railroad Company, recorded March 1, 1898 in Book 203 of Deeds, Page 442, Santa Clara County Records, with the westerly line of Lawrence Station Road (50 feet wide); thence south along said westerly line, 12.48 feet to a point in the westerly prolongation of a line parallel with and distant 26.0 feet southerly, measured at right angles, from the southerly line of that certain 0.141 of an acre parcel of land described in deed dated December 12, 1983 from Southern Pacific Transportation Company to Johnson & Johnson, recorded December 23, 1983 as Instrument No. 7930952, Book I 180 of Official Records, at Page 30, Santa Clara County Records; thence South $89^{\circ} 55' 40''$ East, along said prolongation and parallel line, 523.86 feet; thence South $65^{\circ} 10' 38''$ West, 126.95 feet to a point in a line concentric with and distant 18.00 feet northwesterly, measured radially, from the center line of that certain drill track diverging northerly from the center line of existing eastbound main track (San Francisco - San Jose); thence southwesterly along said concentric line on a curve to the right having a radius of 555.14 feet and a central angle of $14^{\circ} 30' 00''$ (tangent to said curve at last mentioned point is last described course) an arc distance of 140.49 feet

Order No. 990006/SC-61 1ST AMENDED

(Legal Description Continued)

to a point of compound curve; thence southwesterly continuing along said concentric line on a curve to the right having a radius of 898.84 feet and a central angle of $6^{\circ} 15' 00''$ (tangent to said curve at last mentioned point bears South $79^{\circ} 40' 38''$ West) an arc distance of 98.05 feet to a point; thence South $85^{\circ} 55' 38''$ West tangent to said curve at last mentioned point, 11.00 feet; thence South $88^{\circ} 27' 03''$ West 76.09 feet to a point in a line concentric with and distant 18.00 feet northerly measured radially, from said center line of eastbound main track; thence westerly along last said concentric line on a curve to the right having a radius of 7621.45 feet and a central angle of $1^{\circ} 54' 25.2''$ (tangent to said curve at last mentioned point bears North $87^{\circ} 45' 42.4''$ West) an arc distance of 253.67 feet; thence North $85^{\circ} 36' 17''$ West 99.88 feet to a point in a line parallel with and distant 18.00 feet northerly, measured at right angles, from said center line of eastbound main track; thence North $85^{\circ} 28' 47''$ West along said parallel line, 357.18 feet; thence North $85^{\circ} 21' 17''$ West, 99.88 feet to a point in a line concentric with and distant 18.00 feet northerly measured radially, from said center line of eastbound main track; thence westerly along last said concentric line on a curve to the right having a radius of 7621.45 feet and a central angle of $3^{\circ} 45' 41.3''$ (tangent to said curve at last mentioned point bears North $85^{\circ} 06' 17.2''$ West) an arc distance of 500.35 feet to a point in said northerly line of land described in said deed dated February 10, 1898; thence North $89^{\circ} 40' 42''$ East along said northerly line, 1214.46 feet to the point of beginning.

Order No. 990006/SC-64 1ST AMENDED

THE LAND REFERRED TO HEREIN IS SITUATED IN THE CITY OF SUNNYVALE, COUNTY OF SANTA CLARA, STATE OF CALIFORNIA AND IS DESCRIBED AS FOLLOWS:

All of that certain piece, parcel, tract or lot of land situate, lying and being at or near Lawrence Station of the Southern Pacific Railroad Company, County of Santa Clara, State of California, and bounded and particularly described as follows:

BEGINNING in the center of the County Road leading to Alviso, at a point which bears South $0^{\circ} 20'$ East, 20 feet from Station 811 + 83.5 of the center line of the Southern Pacific Railroad; thence running parallel to said center line and distant 20 feet southerly therefrom, South $89^{\circ} 15'$ West, 617.5 feet being bounded by the right of way line of said Southern Pacific Railroad; thence South $74^{\circ} 46'$ East, 243 feet being bounded on the right by a right of way 16 feet wide to Hayshed and Brick Warehouse; thence South $77^{\circ} 02'$ East, 54 feet; thence South $80^{\circ} 24'$ East, 54 feet; thence South $87^{\circ} 28'$ East, 18.5 feet; thence leaving the right of way last mentioned North $1^{\circ} 30'$ East, 20 feet; thence North $87^{\circ} 11'$ East, 259 feet to the center of the County Road aforesaid thence along the center of said road, North $0^{\circ} 20'$ West, 86.5 feet to the point of beginning.

Being the same lands as conveyed by A.C. Lawrence to Southern Pacific Railroad Company, a corporation, by deed recorded April 22, 1876 in Book 39 of Deeds, Page 626, Santa Clara County Records.

EXCEPTING THEREFROM that portion of the above described property lying westerly of the northerly prolongation of the course "North $1^{\circ} 30'$ East 20 feet" as set forth in the above described property.

ALSO EXCEPTING THEREFROM that portion lying within the parcel of land conveyed to the County of Santa Clara, by deed recorded October 14, 1963 in Book 6230 of Official Records, at Page 60, Santa Clara County Records, described as follows:

BEGINNING at a point in the centerline of Lawrence Station Road, formerly a road to Alviso, marking the southeast corner of said land described in said deed; thence northwesterly, along the southerly line of said land as following: North $87^{\circ} 11'$ West, 260.70 feet, shown as 259 feet in said deed; South $1^{\circ} 30'$ West, 20.0 feet; North $87^{\circ} 26'$ West, 18.50 feet and North $80^{\circ} 24'$ West, 24.00 feet; thence northeasterly, leaving said southerly line of said land, along a curve to the right having a radius of 87.10 feet, through an angle of $69^{\circ} 58' 24''$, the chord thereof bears North $58^{\circ} 11' 48''$ East, 99.88 feet, an arc distance of 106.37 feet to a point; thence South $86^{\circ} 49'$ East, tangent to last described curve at last mentioned point, 218.50 feet to a point in the aforesaid center line of Lawrence Station Road; thence South, shown as North $0^{\circ} 20'$ West, in said deed, 38.15 feet to the point of beginning.

Order No. 990006/SC-66 2ND AMENDED

THE LAND REFERRED TO HEREIN IS SITUATED PARTLY IN THE CITY OF SUNNYVALE AND PARTLY IN THE CITY OF SANTA CLARA, COUNTY OF SANTA CLARA, STATE OF CALIFORNIA AND IS DESCRIBED AS FOLLOWS:

All that certain lot, piece or parcel of land situate, lying and being in the Enright-Tract, County of Santa Clara, State of California, and bounded and particularly described as follows:

BEGINNING at the point where the westerly line of property formerly owned by Margaret D. Enright is intersected by the southerly line of the right of way of the Southern Pacific Railroad, said point being situate twenty (20) feet southerly at right angles from the located center line of said railroad at or near engineer's station 813 + 85 of said center line; thence easterly along said southerly line of right of way parallel with said center line, a distance of sixty-two hundred and sixty-five (6265) feet, more or less, to an intersection with the easterly line of said property, at a point situate twenty (20) feet southerly at right angles from said center line at or near engineer's station 876 + 50 of said center line; thence southeasterly along said easterly line of property to a point situate one hundred and twenty (120) feet, southerly at right angles from said center line; thence westerly, parallel with said center line, a distance of twenty-five hundred and forty-four (2544) feet, more or less, to an intersection with the center line of a road crossing at a point situate one hundred and twenty (120) feet southerly at right angles from said center line of said railroad at or near engineer's station 851 + 06 of said center line of railroad; thence northerly along said center line of road crossing to a point situate sixty (60) feet southerly at right angles from said center line of railroad; thence westerly, parallel with said center line of railroad, a distance of thirty-seven hundred and twenty-one (3721) feet, more or less, to an intersection with said westerly line of property at a point situate sixty (60) feet southerly at right angles from said center line of railroad; and thence northerly along said westerly line of property to the point of beginning.

Being the same lands as conveyed by Margaret D. Enright to Southern Pacific Railroad Company, a corporation, by deed recorded January 8, 1903 in Book 260 of Deeds, Page 388, Santa Clara County Records.

EXCEPTING THEREFROM that portion conveyed to Western Projects Corporation, by deed recorded January 18, 1955 in Book 3061 of Official Records, at Page 166, Santa Clara County Records, described as follows:

COMMENCING at an angle point in the northerly line of Tract No. 1309 as delineated on a certain map entitled "Tract No. 1309 Green Vale Manor," Sheet 2, recorded May 17, 1954 in Book 50 of Maps, Pages 10 to 13, inclusive, Santa Clara County Records, said angle point being in the southerly line of land described

PK
8-2-91

Order No. 990006/SC-66 2ND AMENDED

V-74 6 12 13

(Legal Description Continued)

in deed dated December 17, 1902 from Margaret D. Enright to Southern Pacific Railroad Company, recorded January 8, 1903 in Book 260 of Deeds at Page 388, Records of said County, and distant North 89° 36' 30" East 3624.5 feet, measured along said southerly line, from the center line of Lawrence Station Road; thence North 89° 36' 30" East along said southerly line, 106.86 feet to an angle point therein; thence South 0° 23' 30" East continuing along said southerly line, 33.59 feet to a point in said northerly line of said Tract No. 1309, and the actual point of beginning of the parcel of land to be described; thence South 72° 56' 30" East along said northerly line, 97.49 feet to a point in said southerly line of land described in said deed dated December 17, 1902; thence South 89° 36' 30" West along said southerly line, 93.00 feet to an angle point therein; thence North 0° 23' 30" West continuing along said southerly line, 29.23 feet to the actual point of beginning.

05-01-91

A-81

C-7

Order No. 990006/SC-143 1st Amended

THE LAND REFERRED TO HEREIN IS SITUATED IN THE CITY OF MOUNTAIN VIEW, COUNTY OF SANTA CLARA, STATE OF CALIFORNIA AND IS DESCRIBED AS FOLLOWS:

An easement as reserved in the deed dated June 19, 1984, from Southern Pacific Transportation Company, a Delaware corporation, to the State of California, recorded July 20, 1984 in Book 1737 of Official Records, at page 250, as Instrument No. 8133579, Santa Clara County Records, over the following described parcel of land:

BEGINNING at the point of intersection of the northerly line of land described in deed dated February 10, 1898 from Mrs. Sarah E. Houston to Southern Pacific Railroad Company, recorded March 1, 1898 in Book 203 of Deeds, Page 442, Santa Clara County Records, with the westerly line of Lawrence Station Road (50 feet wide); thence south along said westerly line, 12.48 feet to a point in the westerly prolongation of a line parallel with and distant 26.0 feet southerly, measured at right angles, from the southerly line of that certain 0.141 of an acre parcel of land described in deed dated December 12, 1983 from Southern Pacific Transportation Company to Johnson & Johnson, recorded December 23, 1983 as Instrument No. 7930952, Book I 180 of Official Records, at Page 30, Santa Clara County Records; thence South 89° 55' 40" East, along said prolongation and parallel line, 523.86 feet; thence South 65° 10' 38" West, 126.95 feet to a point in a line concentric with and distant 18.00 feet northwesterly, measured radially, from the center line of that certain drill track diverging northerly from the center line of existing eastbound main track (San Francisco - San Jose); thence southwestwardly along said concentric line on a curve to the right having a radius of 555.14 feet and a central angle of 14° 30' 00" (tangent to said curve at last mentioned point is last described course) an arc distance of 140.49 feet to a point of compound curve; thence southwestwardly continuing along said concentric line on a curve to the right having a radius of 898.84 feet and a central angle of 6° 15' 00" (tangent to said curve at last mentioned point bears South 79° 40' 38" West) an arc distance of 98.05 feet to a point; thence South 85° 55' 38" West tangent to said curve at last mentioned point, 11.00 feet; thence South 88° 27' 03" West 76.09 feet to a point in a line concentric with and distant 18.00 feet northerly measured radially, from said center line of eastbound main track; thence westerly along last said concentric line on a curve to the right having a radius of 7621.45 feet and a central angle of 1° 54' 25.2" (tangent to said curve at last mentioned point bears North 87° 45' 42.4" West) an arc distance of 253.67 feet; thence North 85° 36' 17" West 99.88 feet to a point in a line parallel with and distant 18.00 feet northerly, measured at right angles, from said center line of eastbound main track; thence North 85° 28' 47" West along said parallel line, 357.18 feet; thence North 85° 21' 17" West, 99.88 feet to a point in a line concentric with and distant 18.00 feet

V-74/16 Pcl's. 9 & 17 CAL-TRANS "LAWRENCE" ^{AK} 10-25-91

Order No. 990006/SC-143 1st Amended

(Legal Description Continued)

northerly measured radially, from said center line of eastbound main track; thence westerly along last said concentric line on a curve to the right having a radius of 7521.45 feet and a central angle of $3^{\circ} 45' 41.3''$ (tangent to said curve at last mentioned point bears North $85^{\circ} 06' 17.2''$ West) an arc distance of 500.35 feet to a point in said northerly line of land described in said deed dated February 10, 1898; thence North $89^{\circ} 40' 42''$ East along said northerly line, 1214.46 feet to the point of beginning.

EXCEPTING THEREFROM that portion lying within Lawrence Station Road, formerly Alviso Road.

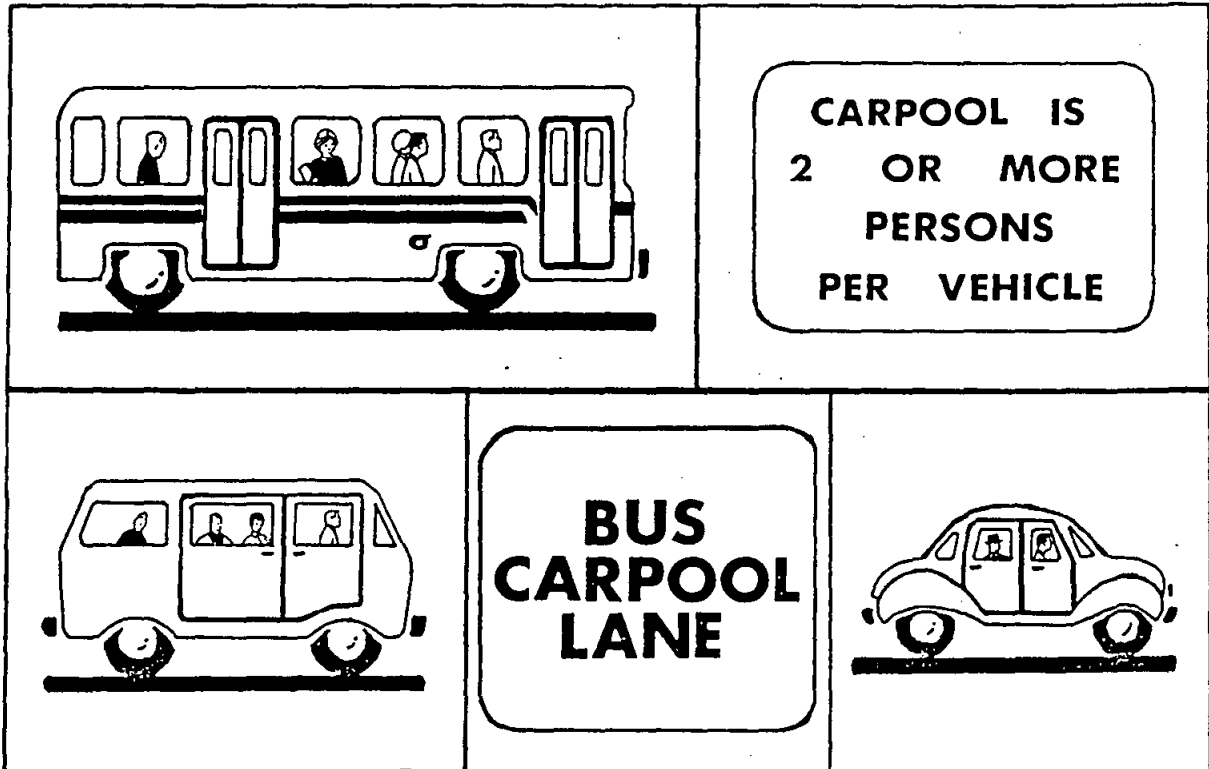
7-31-91

A-78

C-9

**INITIAL STUDY and NEGATIVE DECLARATION (CEQA)
ENVIRONMENTAL ASSESSMENT (NEPA)
and FINDING OF NO SIGNIFICANT IMPACT**

**LAWRENCE EXPRESSWAY
COMMUTER LANES PROJECT**



Santa Clara County, California

**Santa Clara County Transportation Agency
California Department of Transportation
Federal Highway Administration**



April 1987

APPROVED BY THE BOARD OF SUPERVISORS
OF SANTA CLARA COUNTY APR 28 1987

DONALD M. RAINS, Clerk of the Board

By Michael J. Ryan
Deputy Clerk

D-1

EXHIBIT D

Proposed Commuter Lanes Project

on

Lawrence Expressway

Santa Clara County, California

ENVIRONMENTAL ASSESSMENT

Santa Clara County Transportation Agency

and

State of California, Department of Transportation

and

U.S. Department of Transportation, Federal Highway Administration

Pursuant to: 42 U.S.C. 4332(2)(C)
23 U.S.C. 128(a)

James E. Reading
James E. Reading, Director
Santa Clara County Transportation Agency

June 30, 1986
Date

Walt Gohl
Walt Gohl
Local Assistance
Caltrans District 04

July 9, 1986
Date

for John R. Schultz
Bruce E. Cannon
Division Administrator
Federal Highway Administration

8-18-86
Date

M E M O R A N D U M

TO: Transportation Agency
FROM: Clerk, Board of Supervisors
DATE: May 20, 1987

On April 28, 1987, the Board of Supervisors

approved a project which had been determined to be exempt from CEQA.

adopted a Negative Declaration.

certified as complete an Environmental Impact Report.

for the following project:

Approved Lawrence Expressway Commuter Lanes Project.

Approved Negative Declaration for the Lawrence Expressway Commuter Lanes Project.

Restated continued support of policy re safe bicycle access to County expressways.

Your agency/department is responsible for the proper filing of a Notice of Exemption or a Notice of Determination setting forth the action taken as provided by Public Resources Code 15062 and 21152. If you have any questions regarding the form of the Notice, where it must be filed, or the procedure to be followed, you may receive advice from the County Counsel's Office, telephone 299-2111.

Please return to the Clerk of the Board a copy of this notice along with a copy of the notice you file with the County Clerk and Secretary for Resources.

DMR:



Transportation Agency

Santa Clara County Bus, Light Rail, Roads, Aviation

#2 8/10/93

3331 North First Street
San Jose, CA 95134-1906

33 AUG 17 P 1: 20

RCV'D
BD OF SUPERVISORS
CO OF SANTA CLARA

August 13, 1993

Dockets Office
State of California Public Utilities Commission
505 Van Ness Avenue
San Francisco, CA 94102

Subject: Application of the County of Santa Clara, Transportation Agency to modify existing grade separated grade crossing.

On August 10, 1993 the County of Santa Clara Board of Supervisors authorized the filing of this application with the California State Public Utilities Commission.

Enclosed are an original and twelve (12) copies of the application for modification of an existing grade separated crossing of Lawrence Expressway by the Peninsula Corridor Joint Powers Board main commuter rail line mile post 40.78 and Southern Pacific spur rail line in the city of Sunnyvale, California.

Under the provisions of General Order No. 88-A, the County of Santa Clara Transportation Agency, requests authority to modify the subject grade crossing.

If you need additional information, please call Kevin D. Allmand at (408) 299-2111 or Gamini Rajapakse at (408) 321-7133.

Sincerely,

Lawrence Reuter
Director

Enclosures

CC: Clerk of the Board of Supervisors
LM, SAB, RBP, JRR, MLG, Law-file

H 93-08-0037
PCA # 3475



Transportation Agency

Santa Clara County Bus, Light Rail, Roads, Aviation

3331 North First Street
San Jose, CA 95134-1906

August 13, 1993

Ms. Sally Reed
County Executive
County of Santa Clara
70 West Hedding Street
San Jose, CA 95110

Subject: CPUC Application of the County of Santa Clara, Transportation Agency
to modify existing grade separated grade crossing.

Dear Ms. Reed:

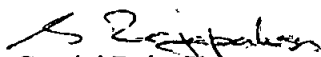
On August 10, 1993 the County of Santa Clara Board of Supervisors authorized the filing of this application with the California State Public Utilities Commission (CPUC).

Enclosed is a copy of the application for modification of an existing grade separated crossing of Lawrence Expressway by the Peninsula Corridor Joint Powers Board main commuter rail line mile post 40.78 and Southern Pacific spur rail line in the city of Sunnyvale, California.

County of Santa Clara, Transportation Agency requests you to acknowledge to CPUC of (a) Receipt of a copy of the application (b) Concurrence to the subject project. Please send your response to, Dockets Office, State of California Public Utilities Commission, 505 Van Ness Avenue, San Francisco, CA 94102.

If you need additional information, please call me at (408) 321-7144.

Sincerely,


Gamini Rajapakse
Project Engineer

Enclosures

CC: RBP, JRR, MLG, Law-file

H 93-08-0040
PCA # C3475



Transportation Agency

Santa Clara County Bus, Light Rail, Roads, Aviation

3331 North First Street
San Jose, CA 95134-1906

August 13, 1993

Mr. Tomas Lewcock
City Manager
City of Sunnyvale
456 West Olive Avenue
Sunnyvale, CA 94088-3707

Subject: CPUC Application of the County of Santa Clara, Transportation Agency
to modify existing grade separated grade crossing.

Dear Mr. Lewcock:

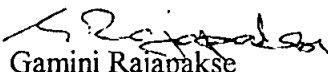
On August 10, 1993 the County of Santa Clara Board of Supervisors authorized the filing of this application with the California State Public Utilities Commission (CPUC).

Enclosed is a copy of the application for modification of an existing grade separated crossing of Lawrence Expressway by the Peninsula Corridor Joint Powers Board main commuter rail line mile post 40.78 and Southern Pacific spur rail line in the city of Sunnyvale, California.

County of Santa Clara, Transportation Agency requests you to acknowledge to CPUC of (a) Receipt of a copy of the application (b) Concurrence to the subject project. Please send your response to, Dockets Office, State of California Public Utilities Commission, 505 Van Ness Avenue, San Francisco, CA 94102.

If you need additional information, please call me at (408) 321-7144.

Sincerely,


Gamini Rajapakse
Project Engineer

Enclosures

CC: RBP, JRR, MLG, Law-file

H 93-08-0039
PCA # C3475



Transportation Agency

Santa Clara County Bus, Light Rail, Roads, Aviation

3331 North First Street
San Jose, CA 95134-1906

August 13, 1993

Mr. Art Aldridge
Safety Division
State of California Public Utilities Commission
505 Van Ness Avenue
San Francisco, CA 94102

Subject: Application of the County of Santa Clara, Transportation Agency to modify existing grade separated grade crossing.

Dear Mr. Aldridge:

On August 10, 1993 the County of Santa Clara Board of Supervisors authorized the filing of this application with the California State Public Utilities Commission.

Enclosed is a copy of the application for modification of an existing grade separated crossing of Lawrence Expressway by the Peninsula Corridor Joint Powers Board main commuter rail line mile post 40.78 and Southern Pacific spur rail line in the city of Sunnyvale, California and Environmental Assessment and Finding of No Significant Impact report. We have sent the original and twelve copies of the application to the Dockets Office.

Under the provisions of General Order No. 88-A, the County of Santa Clara Transportation Agency, requests authority to modify the subject grade crossing.

If you need additional information, please call me at (408) 321-7133.

Sincerely,

A handwritten signature in black ink, appearing to read 'G. Rajapakse'.

Gamini Rajapakse
Project Engineer

Enclosures

CC: RBP, JRR, MLG, Law-file

H 93-08-0043
PCA # 3475



Transportation Agency

Santa Clara County Bus, Light Rail, Roads, Aviation

3331 North First Street
San Jose, CA 95134-1906

August 13, 1993

Mr. C.J. Burroughs
Chief Engineer
Southern Pacific Transportation Company
One Market Plaza, Room 1001
San Francisco, CA 94105

Attn: Mr. Dick Dahllof

Subject: CPUC Application of the County of Santa Clara, Transportation Agency
to modify existing grade separated grade crossing.

Dear Mr. Burroughs:

On August 10, 1993 the County of Santa Clara Board of Supervisors authorized the filing of this application with the California State Public Utilities Commission (CPUC).

Enclosed is a copy of the application for modification of an existing grade separated crossing of Lawrence Expressway by the Peninsula Corridor Joint Powers Board main commuter rail line mile post 40.78 and Southern Pacific spur rail line in the city of Sunnyvale, California.

County of Santa Clara, Transportation Agency requests you to acknowledge to CPUC of (a) Receipt of a copy of the application (b) Concurrence to the subject project. Please send your response to, Dockets Office, State of California Public Utilities Commission, 505 Van Ness Avenue, San Francisco, CA 94102.

If you need additional information, please call me at (408) 321-7144.

Sincerely,

Gamini Rajapakse
Project Engineer

Enclosures

CC: RBP, JRR, MLG, Law-file

H 93-08-0038
PCA # C3475



3331 North First Street
San Jose, CA 95134-1906

August 13, 1993

Mr. Tom Davids
Property Manager
Peninsula Corridor Joint Powers Board
1250 San Carlos Avenue
San Carlos, CA 94070-1306

Subject: CPUC Application of the County of Santa Clara, Transportation Agency
to modify existing grade separated grade crossing.

Dear Mr. Davids:

On August 10, 1993 the County of Santa Clara Board of Supervisors authorized the filing of this application with the California State Public Utilities Commission (CPUC).

Enclosed is a copy of the application for modification of an existing grade separated crossing of Lawrence Expressway by the Peninsula Corridor Joint Powers Board main commuter rail line mile post 40.78 and Southern Pacific spur rail line in the city of Sunnyvale, California.

County of Santa Clara, Transportation Agency requests you to acknowledge to CPUC of (a) Receipt of a copy of the application (b) Concurrence to the subject project. Please send your response to, Dockets Office, State of California Public Utilities Commission, 505 Van Ness Avenue, San Francisco, CA 94102.

If you need additional information, please call me at (408) 321-7144.

Sincerely,


Gamini Rajapakse
Project Engineer

Enclosures

CC: RBP, JRR, MLG, Law-file

H 93-08-0042
PCA # C3475



Transportation Agency

Santa Clara County Bus, Light Rail, Roads, Aviation

3331 North First Street
San Jose, CA 95134-1906

August 13, 1993

Mr. Preston W. Kelly
District IV, Director
State of California
Department of Transportation
P O Box 23660
Oakland, CA 94623-0660

Subject: CPUC Application of the County of Santa Clara, Transportation Agency
to modify existing grade separated grade crossing.

Dear Mr. Kelley:

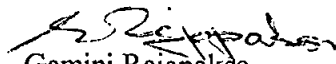
On August 10, 1993 the County of Santa Clara Board of Supervisors authorized the filing of this application with the California State Public Utilities Commission (CPUC).

Enclosed is a copy of the application for modification of an existing grade separated crossing of Lawrence Expressway by the Peninsula Corridor Joint Powers Board main commuter rail line mile post 40.78 and Southern Pacific spur rail line in the city of Sunnyvale, California.

County of Santa Clara, Transportation Agency requests you to acknowledge to CPUC of (a) Receipt of a copy of the application (b) Concurrence to the subject project. Please send your response to, Dockets Office, State of California Public Utilities Commission, 505 Van Ness Avenue, San Francisco, CA 94102.

If you need additional information, please call me at (408) 321-7144.

Sincerely,


Gamini Rajapakse
Project Engineer

Enclosures

CC: RBP, JRR, MLG, Law-file

H 93-08-0041
PCA # C3475



Transportation Agency

Santa Clara County Bus, Light Rail, Roads, Aviation

3331 North First Street
San Jose, CA 95134-1906

8/10/93 #8
RCV'D
BD OF SUPERVISORS
CO OF SANTA CLARA

14 JAN 6 P 3: 12

January 6, 1994

Ms. Phyllis A. Perez
Clerk of the Board of Supervisors
County of Santa Clara
70 West Hedding Street
San Jose, CA 95110

Subject: Approved Application of the County of Santa Clara, Transportation Agency
to Modify the Lawrence Expressway, Existing Grade Separated Crossing

Dear Ms. Perez:

On August 10, 1993 the County of Santa Clara Board of Supervisors authorized the filing of the subject application with the California State Public Utilities Commission (CPUC).

Attached please find a copy of the approved CPUC application (93-08-031) for filing with your office.

If you have any questions please call me at (408) 321 - 7144.

Sincerely,

GAMINI RAJAPAKSE
Project Engineer

Attachment

CC: Kevin Allmand, Deputy County Counsel
RBP, JRR, MLG, ACD, JLK, File-Law, Rec. Mgmt

H94-01-0025
PCA#C3475

Decision 93-12-041 December 17, 1993

BEFORE THE PUBLIC UTILITIES COMMISSION OF THE STATE OF CALIFORNIA

Application of the County of Santa Clara)	
for an order authorizing modification)	
of a grade separated crossing of)	Application 93-08-031
Lawrence Expressway by the Peninsula)	(Filed August 17, 1993)
Corridor Joint Powers Board main)	
commuter rail line mile post 40.78 and)	
Southern Pacific spur rail line in the)	
City of Sunnyvale, California.)	

O P I N I O N

As part of a project to construct two High Occupancy Vehicle Lanes (HOV) on the Lawrence Expressway between Prospect Road in San Jose and State Route (SR) 237 in Sunnyvale, the Santa Clara County Transportation Agency (SCCTA) requests authority to widen the Lawrence Expressway at the "Sunnyvale (SPRR) Overhead" at separated grades over the main line tracks of Peninsula Corridor Joint Powers Board (PCJPB) and Southern Pacific Transportation Company's (SPT) spur tracks in Sunnyvale, Santa Clara County.

The eight mile long project will alleviate current and projected peak hour traffic congestion on Lawrence Expressway, and will improve traffic connections between State Route 237 and Interstate 280. The proposed HOV lanes will be on the outside lanes, and will be in use only during peak commute periods. During all other times, the HOV lanes will function as shoulders of the expressway.

The Sunnyvale (SPRR) Overhead is located in the northern portion of the widening project, just south of Central Expressway. The overhead will be widened only on the east side of the structure, and the bridge centerline will be shifted to accommodate HOV lanes in both directions of travel.

SCCTA is the lead agency for this project under the California Environmental Quality Act of 1970 (CEQA), as amended,

Public Resources Code Sections 21000, et seq. After preparation and review of an Initial Study, SCCTA issued a Negative Declaration and approved the project. On April 28, 1987, a Notice of Determination was filed with the Office of Planning and Research which found that "The project will not have a significant effect on the environment."

The Commission is a responsible agency for this project under CEQA and has reviewed and considered the lead agency's Initial Study and Negative Declaration.

The Commission's Safety Division Traffic Engineering staff inspected the site of the proposed project. After reviewing the need for and safety of the proposed construction, the staff recommends issuance of an ex parte order authorizing widening of the overhead.

The application meets the filing requirements of the Commission's Rules of Practice and Procedure, including Rule 39, which relates to the widening of an existing crossing. A sketch of the crossing area is included as Appendix A.

Findings of Fact

1. Notice of the application was published in the Commission's Daily Calendar on August 25, 1993. No protests have been received. A public hearing is not necessary.
2. SCCTA requests authority under Public Utilities Code Sections 1201-1205 to widen the Sunnyvale (SPRR) Overhead at separated grades over the tracks of PCJPB mainline and SPT's spur tracks in Sunnyvale, Santa Clara County.
3. Widening of the overhead is required to improve roadway capacity and reduce congestion.
4. Public convenience, necessity, and safety require widening of the overhead.
5. SCCTA is the lead agency for this project under CEQA, as amended.

6. The Commission is a responsible agency for this project and has reviewed and considered the lead agency's Initial Study and Negative Declaration.

7. The project will not have a significant effect on the environment.

Conclusion of Law

The application should be granted as set forth in the following order.

O R D E R

IT IS ORDERED that:

1. The County of Santa Clara, Transportation Agency (SCCTA), is authorized to widen Lawrence Expressway at the Sunnyvale (SPRR) Overhead at separated grades over the tracks of Peninsula Corridor Joint Powers Board (PCJPB) and Southern Pacific Transportation Company's (SPT) spur tracks in Sunnyvale, Santa Clara County, at the location and substantially as shown by plans attached to the application, identified as Crossing E-40.7-A.

2. Clearances shall be in accordance with General Order (GO) 26-D.

3. Walkways shall conform to GO 118. Walkways adjacent to any trackage subject to rail operations shall be maintained free of obstructions and shall be promptly restored to their original condition in the event of damage during construction.

4. Construction and maintenance costs shall be borne in accordance with an agreement to be entered into between the parties. A copy of the agreement, together with plans of the crossing approved by PCJPB and SPT, shall be filed with the Commission's Safety Division prior to commencing construction. Should the parties fail to agree, the Commission will apportion the costs of construction and maintenance by further order.

5. Within 30 days after completion of the work under this order, SCCTA shall advise the Commission's Safety Division in writing that the authorized work has been completed.

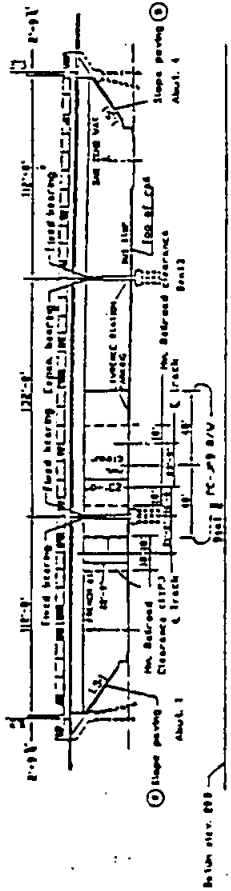
6. This authorization shall expire if not exercised within three years unless time is extended or if the above conditions are not complied with. Authorization may be revoked or modified if public convenience, necessity, or safety so require.

7. The application is granted as set forth above.

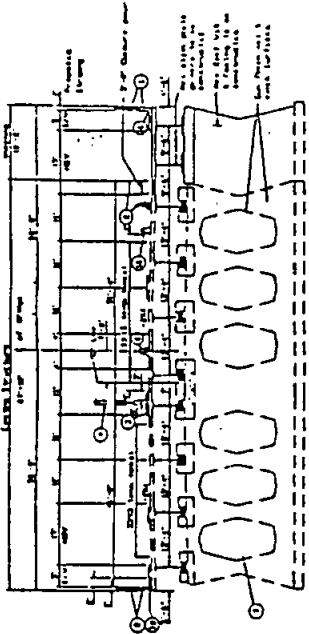
This order becomes effective 30 days from today.

Dated December 17, 1993, at San Francisco, California.

DANIEL Wm. FESSLER
President
PATRICIA M. ECKERT
NORMAN D. SHUMWAY
P. GREGORY CONLON
JESSIE J. KNIGHT, Jr.
Commissioners



ELEVATION
1"=30"



LATERAL SECTION
1"=10"

LEGEND:

1. All dimensions are in feet and inches unless otherwise noted.

2. All structural steel is A36 steel unless otherwise noted.

3. All structural concrete is 3000 psi concrete unless otherwise noted.

4. All reinforcement is #4 bars unless otherwise noted.

5. All reinforcement is placed in accordance with ACI 318-63.

6. All dimensions are given to the centerline of the member unless otherwise noted.

7. All dimensions are given to the finished surface unless otherwise noted.

8. All dimensions are given to the top surface unless otherwise noted.

9. All dimensions are given to the bottom surface unless otherwise noted.

10. All dimensions are given to the side surface unless otherwise noted.

11. All dimensions are given to the top surface of the member unless otherwise noted.

12. All dimensions are given to the bottom surface of the member unless otherwise noted.

13. All dimensions are given to the side surface of the member unless otherwise noted.

14. All dimensions are given to the top surface of the member unless otherwise noted.

15. All dimensions are given to the bottom surface of the member unless otherwise noted.

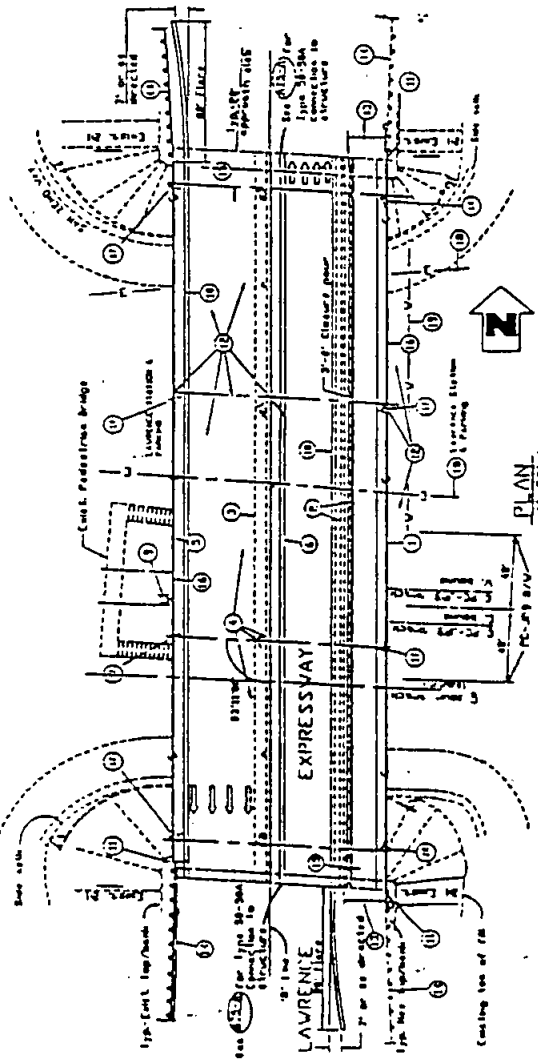
16. All dimensions are given to the side surface of the member unless otherwise noted.

17. All dimensions are given to the top surface of the member unless otherwise noted.

18. All dimensions are given to the bottom surface of the member unless otherwise noted.

19. All dimensions are given to the side surface of the member unless otherwise noted.

20. All dimensions are given to the top surface of the member unless otherwise noted.



3077 DESIGN PLAN



Transportation Agency

Santa Clara County Bus, Light Rail, Roads, Aviation

3331 North First Street
San Jose, CA 95134-1906

BY THE BOARD OF SUPERVISORS
OF THE COUNTY OF SANTA CLARA

Phyllis A. Poraz, Clerk of the Board

By

Deputy Clerk

Date: AUG 10 1993

S.D. 3 & 4

TRANSMITTAL MEMORANDUM

PAGE 1 of 2

Prepared by Rajapakse
Reviewed by Randall/Parsons
Submitted by Bruce
APPROVED: DIRECTOR [Signature]

DATE: July 20, 1993

TRANSIT DISTRICT BOARD:	Agenda Date _____	Item No. _____
COUNTY BOARD OF SUPERVISORS:	Agenda Date <u>August 10, 1993</u>	Item No. _____
TRANSPORTATION COMMISSION:	Agenda Date _____	Item No. _____

FROM: Scotty A. Bruce, Deputy Director
Highways Design & Operations

SUBJECT: SUNNYVALE (SPRR) OVERHEAD/LAWRENCE EXPRESSWAY COMMUTER (HOV) LANE
PROJECT APPLICATION TO THE CALIFORNIA PUBLIC UTILITY COMMISSION (CPUC)
FOR GRADE SEPARATED CROSSING MODIFICATION (BRIDGE WIDENING)

RECOMMENDED ACTION:

1. Approve the filing of an application before the California Public Utilities Commission (CPUC) for their approval of the bridge widening construction on Lawrence Expressway.
2. Authorize County Counsel and the Transportation Agency Director to execute and file the applicable documents with CPUC.

FISCAL IMPLICATIONS:

There are no fiscal implications from this application process.

REASONS FOR RECOMMENDATION:

Any rail crossing, at-grade or grade separated, is under the jurisdiction of the CPUC, and CPUC approval must be secured for any type of modification or replacement of the crossing (Section 1201 of the California Public Utilities Code). The Lawrence Expressway Commuter (HOV) Lane Project will widen the Sunnyvale Overhead bridge over the Peninsula Corridor Joint Powers Board (PC-JPB) main commuter rail line and Southern Pacific rail line, therefore an application must be submitted to the CPUC for approval.

CONSEQUENCES OF NEGATIVE ACTION:

There can be no Commuter (HOV) lane construction on the Sunnyvale Overhead structure without CPUC approval. The Project could not be completed.

MKA

ORIGINAL

H93070053

Date: July 20, 1993

COUNTY BOARD OF SUPERVISORS: Agenda Date: August 10, 1993 Item: _____

SUBJECT: SUNNYVALE (SPRR) OVERHEAD/LAWRENCE EXPRESSWAY COMMUTER (HOV) LANE
PROJECT APPLICATION TO THE CALIFORNIA PUBLIC UTILITY COMMISSION (CPUC)
FOR GRADE SEPARATED CROSSING MODIFICATION (BRIDGE WIDENING)

STEPS FOLLOWING APPROVAL:

The application and relevant documents will be executed by County Counsel and/or the Director of the Transportation Agency. The Transportation Agency will file the original and twelve (12) copies of the application and relevant documents with the California Public Utilities Commission.

A copy of the application is to be mailed by the Transportation Agency to the parties listed on the Proof of Service by Mail.

SAB:GR:lr
[2143]

Attachments